

PLANNING COMMISSION AGENDA ITEM
CITY OF SHORELINE, WASHINGTON

AGENDA TITLE:	Second Public Hearing on Town Center Subarea Plan and Development Code
DEPARTMENT:	Planning and Development Services
PRESENTED BY:	Joseph W. Tovar, FAICP, Director Paul Cohen, Senior Planner

I. RECOMMENDATION

The Planning Commission will hold a second public hearing on June 2 regarding the Town Center Subarea Plan and Code. Since the close of the first public hearing held May 5, 2011, the Draft Supplemental EIS was issued. (A link to the DSEIS was sent to the Commission earlier this month.) The public is invited to make comments regarding the Draft SEIS at the June 2 hearing; however, written comments will be accepted through June 9. This staff report addresses proposed policy and code changes that the Planning Commission asked staff to address at the May 5 hearing.

Staff recommends that the June 2 hearing be continued to the June 16 meeting to allow the Commission to review all written comments on the Draft SEIS and so that the staff can draft potential amendments that you might suggest. At the June 16 meeting, the Commission would close the public hearing, deliberate and vote on amendments to the draft Subarea Plan and Code, and forward its recommendation to the City Council.

II. BACKGROUND

The following background information is repeated from the May 5 public hearing to provide information to members of the public who may not have been in attendance then.

A. Policy Direction from City Council

The City Council, in its Goals for 2010-2011, has identified as a major priority adoption of amendments to the Comprehensive Plan such as the Town Center Subarea Plan, as well as creating permit processes that are more timely and predictable. The specific goal, overview and objectives are as follows:

Goal 1 -- Implement the adopted Community Vision by updating the Comprehensive Plan and key development regulations in partnership with residents, neighborhoods, and businesses.

Approved By:

Project Manager PLC

Planning Director ____

Goal Overview: To implement the adopted Community Vision, which integrates the Environmental Sustainability, Housing and Economic Development Strategies with citizen input received at the vision and values workshops conducted in 2008. This will be accomplished through drafting various elements of the Comprehensive Plan, Southeast Neighborhoods Subarea Plan, Town Center Subarea Plan, new tree regulations and permit process adjustments.

Major Objectives:

- Adopt the Town Center Subarea Plan
- Make the permit process clear, timely and predictable through Subarea Plans, Planned Actions and other appropriate planning tools

The staff and Planning Commission have attempted to embody this policy direction in the current draft of the Town Center Subarea Plan and Code. The overall objective is to create an attractive, compact, walkable and mixed-use center that meets the City's goals for economic and environmental sustainable, as well as social equity.

The Subarea plan capitalizes on the area's close-in regional location and good transit service to create a focal point for much of the City's future commercial and residential growth. At the same time, the Town Center Subarea Plan seeks to connect, respect and protect the single family neighborhoods that adjoin it immediately to the east and west of the Aurora corridor.

The Town Center Code places a greater emphasis on regulating the three-dimensional form and visual character of new development, as opposed to traditional zoning, which usually focused in more detail on the specific land uses within those forms. The site and building design standards are illustrated to clearly convey to project applicants the form and character that the community wishes to see in Town Center development.

The Town Center Code also places a greater focus on the relationship and orientation of buildings to the public realm of adjoining streets, walkways and public spaces. In this way, the Code seeks to provide greater certainty about the actual form and character of what gets built while making the City's decision-making process attractive for future investment decisions. Likewise, by adopting the Town Center Code as a SEPA Planned Action, the environmental review of new projects can be greatly expedited and folded into the administrative design review process which is another innovative process feature.

B. Public outreach and the evolution of the proposals

Creating a Town Center for Shoreline has been a perennial topic for the City since incorporation in 1995.

- In 1996 the City Council identified this area as a commercial and civic center in their Visioning Map.
- In a 1998 community exercise, citizens identified the general area around N.175th and Aurora Avenue as the “Heart of Shoreline”.
- In 2003 the Planning Commission recommended that the City adopt the Central Shoreline Subarea Plan, which roughly corresponded to the present boundaries of the proposed Town Center.

After a four year hiatus, the City launched its current Town Center planning efforts. As a courtesy and as a matter of practice, the City sent notification of meetings via the city website, Facebook, Currents newsletter, mailing list, Shoreline Area News, adjoining neighborhood associations, commerce organizations, and 6 signboards in the Town Center. Required public hearing notices were sent to the Seattle Times and to property owners in the Town Center

- In 2007 the City launched the effort with:
 - Council adoption of five Town Center framework goals for the Comprehensive Plan.
 - Council adoption of thirteen interim Strategic Points to help provide context for the design work of two major public projects in Town Center: Mile 2 of the Aurora Project and the new City Hall.
 - A Shoreline Speaker Series featured regional and national experts on planning and design issues, still on the City’s website as streaming video at <http://shorelinewa.gov/index.aspx?page=181>
 - February: Mark Hinshaw on “Housing and Demographic Trends and Innovative Responses.”
 - March: Gene Duvernoy on “Cascade Agenda Cities.”
 - April: Amalia Leighton on “Creating and Implementing Green Infrastructure.”
 - May: Ron Sher on “A Discussion of Community Gathering Places.”
 - August: Dan Burden on “Walkable and Pedestrian friendly communities.”
- May 2008, the Planning Commission hosted a design charrette with the public to inventory existing conditions and brainstorm possible future design themes.
- April 2009, the City Council adopted a Citywide Vision that identifies the lands along Aurora Avenue N between N 175th and 185th Streets as the core of an emerging Town Center.
- July 2009 – Town Center walkabout with the Planning Commission and interested citizens.
- August 2009 – Planning Commission reviewed synthesis of past relevant actions and projects, updates on the Aurora project, economic development potential and the public outreach program for the Town Center effort.

- October 2009 – Planning Commission hosted a Town Center Open House to review preliminary land use, circulation and design concepts with the public.
- October 2009 – City launched Town Center Subarea Facebook Page at <https://www.facebook.com/ShorelineTownCenter>.
- October 2009 – City launched online Town Center issue survey (250 respondents).
- January 2010 – Planning Commission workshop to discuss project background, transportation issues and coordination with the Transportation Master Plan.
- April 2010 – Town Center Charrette with the public to explore form and design issues in Town Center and vote in a visual preference survey. The survey continued online, city-wide (250 respondents).
- June 2010 – Staff workshop with Richmond Highlands and Meridian Park Neighborhood Associations to solicit input on ways to connect and protect residential neighborhoods adjacent to Town Center.
- July and August 2010 – Planning Commission meetings to review results of public outreach and preliminary discussion of design standards for inclusion in Town Center Code.
- September 2010 – Reviewed draft of Town Center Subarea Plan and Code with Planning Commission.
- January 2011 – Draft Town Center Subarea Plan and Code posted to City website and released for public review.
- April 2011 – Planning Commission study session to hear staff update on preliminary public comment on draft Subarea Plan and Code.

III. ANALYSIS OF THE PROPOSED ACTIONS

The proposed Town Center Subarea Plan and Code have been reviewed pursuant to the requirements of the State Environmental Policy Act (SEPA), RCW 43.21C. The Planning Commission will also review the actions for compliance with the provisions of SMC 20.30.340 as to the subarea plan and SMC 20.30.350 as to the Development Code. These analyses are presented below.

A. SEPA

On May 10, the City issued a Draft SEIS to evaluate the likely environmental consequences of the Town Center Subarea Plan and Town Center Code. The City proposes to adopt these documents as a SEPA “Planned Action”, per RCW 43.21C.031. The purpose of a Planned Action is to shift environmental review forward from the time a permit application is made to an earlier phase in the development process.

As such, future development proposals within the Town Center boundaries that are consistent with the development assumptions of the subarea would be exempt from SEPA. Future projects would be subject to all applicable codes and regulations.

A link to the Draft SEIS (DSEIS) was sent to the Commission when the DSEIS was released. The transmittal letter that accompanied the DSEIS explained the methodology

used to prepare the document, as well as the mechanics of how the Planned Action review would occur.

B. Comprehensive Plan Amendment Criteria – 20.30.340

1. The amendment is consistent with the Growth Management Act and not inconsistent with the Countywide Planning Policies, and the other provisions of the Comprehensive Plan and City policies; or

The amendment is supported by state, county, and city goals and policies. The GMA supports urban growth, reducing sprawl, economic development, housing, transportation, and citizen participation. Countywide policy FW-11 supports concentration of development in urban growth areas and accommodation of the 20-year population projection and employment forecast including to promote a land use pattern than can be served by public transportation.

Several Shoreline Comprehensive Plan policies support the amendments, including

- *Goal LU VIII – Change the Aurora Corridor from a commercial strip to distinctive centers with variety, activity, and interest by:*

Balancing vehicle, transit, and pedestrian needs

Creating a “sense of place” and improving image for each center

Protecting neighborhoods

Encouraging thriving businesses

Using sound marketing principles

- *Policy LU25: Pursue opportunities to improve the City’s image by creating a sense of place on the Aurora Corridor for doing business and attracting retail activity.*
- *Policy LU27: Ensure street design and urban design is distinctive in the center part on the Aurora Corridor, from 175th through 185th.*

2. The amendment addresses changing circumstances, changing community values, incorporates a sub area plan consistent with the Comprehensive Plan vision or corrects information contained in the Comprehensive Plan; or

- *2009 Shoreline City-wide Vision Statement and Framework Goals:*

“You’ll also find safe, well-maintained bicycle routes that connect all of the main streets to each other and to the Aurora core area, as well as convenient

and reliable local bus service throughout the day and throughout the city. If you live nearby, sidewalks connect these hubs of activity to the surrounding neighborhood, bringing a car-free lifestyle within reach for many.”

“Aurora Avenue is Shoreline’s grand boulevard. It is a thriving corridor, with a variety of shops, businesses, eateries and entertainment, and includes clusters of some mid-rise buildings, well-designed and planned to transition to adjacent residential neighborhoods gracefully. Shoreline is recognized as a business-friendly city. Most services are available within the city, and there are many small businesses along Aurora, as well as larger employers that attract workers from throughout the region. Here and elsewhere, many Shoreline residents are able to find family-wage jobs within the City.”

“As you walk down Aurora you experience a colorful mix of bustling hubs – with well-designed buildings, shops and offices – big and small – inviting restaurants, and people enjoying their balconies and patios. The boulevard is anchored by the vibrant Town Center, which is focused between 175th and 185th Street. This district is characterized by compact, mixed-use, pedestrian-friendly development highlighted by the Shoreline City Hall, the Shoreline Historical Museum, Shorewood High School, and other civic facilities. The interurban park provides open space, recreational opportunities, and serves as the city’s living room for major festivals and celebrations.”

- *FG 2: Provide high quality public services, utilities, and infrastructure that accommodate anticipated levels of growth, protect public health and safety, and enhance the quality of life.*
 - *FG 4: Provide a variety of gathering places, parks, and recreational opportunities for all ages and expand them to be consistent with population changes.*
 - *FG 9: Promote quality building, functionality, and walkability through good design and development that is compatible with the surrounding area.*
 - *FG 10: Respect neighborhood character and engage the community in decisions that affect them.*
 - *FG 14: Designate specific areas for high density development, especially along major transportation corridors.*
3. The amendment will benefit the community as a whole, will not adversely affect community facilities, the public health, safety or general welfare. (Ord. 238 Ch. III § 7(f), 2000).

The amendment will benefit the community by focusing much of future growth into a district that is best served by transit, pedestrian circulation, is central to Shoreline, and provides centralized, convenient pedestrian access to public events, amenities and services.

C. Development Code Amendment Criteria – 20.30.350

1. The amendment is in accordance with the Comprehensive Plan; and

See Comprehensive Plan Amendment Criteria above.

2. The amendment will not adversely affect the public health, safety or general welfare; and

The amendment is planned to mitigate potential impacts while providing a safe, attractive, pedestrian accessible district that directs most of the future growth away from the single family neighborhoods.

3. The amendment is not contrary to the best interest of the citizens and property owners of the City of Shoreline. (Ord. 238 Ch. III § 7(g), 2000).

The amendment is consistent with the majority of the citizens and property owners who commented, were surveyed, and participated in Town Center community meetings. The major intent is to provide a neighborhood district for the entire city that encourages economic vitality and serves nearby neighborhoods and the community.

PLANNING COMMISSION AND PUBLIC COMMENT TO DATE

A. Items Discussed at the May 5 Meeting

1. Computer Videos - Staff will present two, short computer animated videos that show how two sites in Town Center might develop under the proposed development code. One project is on the Van Guard / Interurban Building site and the other is on the Fred Meyer / Highland Ice Arena site. Neither rendering represents actual proposals by the property owners to develop the site; rather they represent two of multiple possibilities assembled by staff using the proposed development code.
2. Green Factor – Staff suggested investigating Seattle's use of a sustainability checklist used in the review of development proposals. *Staff reviewed Seattle's checklist and found it to be redundant to Shoreline's current and proposed standards and too complicated. Staff's conclusion is that its adoption would result in a disincentive to develop in Town Center.* Shoreline currently has or will soon have codes that promote key sustainability issues. The proposed Town Center regulations include the only tree preservation requirement in a commercial area, 2005 storm and surface water standards that have update Low Impact Development requirements, and parking reduction criteria along Aurora that encourage less use of the automobile. In addition, staff anticipates the adoption of the International Green Building Code in 2012 that would be applicable city-wide. Town Center Policy TC18 directly supports adoption of this

building code. As Bus Rapid Transit begins serving the area in 2012, staff expects that more services and activities will locate in Town Center and its reputation as a sustainable place will only increase.

3. Survey of Development Thresholds – Staff researched development thresholds from the jurisdictions of Mill Creek, Covington, Kirkland, Mountlake Terrace, and Bothell. None of these jurisdictions have development thresholds for when full site improvements are required.

Transition Overlay – Some Planning Commissioners raised questions about the proposed Transition Overlay between commercial and single family residential. They requested additional information to help them decide whether the current proposal is equitable and provides enough protection in various scenarios. Staff defined two different scenarios to show how the overlay would work where Town Center boundaries 1. abut or 2. are across a right-of-way from single family zones (Attachment 3). *Staff recommends two types of transition overlays that have different yet equitable standards.*

One comparison to consider is that in both scenarios a 125-foot separation is maintained between the maximum heights of single family homes adjacent to Town Center and the maximum height of buildings in the Town Center.

Another comparison (same scenarios) is the horizontal separation of building facades (facing property line) with different types of buffers in between. Transition Overlay-1 has 35 horizontal feet between two, 35 foot high facades with a 20-foot, vegetative screen between. (As a reference, two single family developments, currently, could have 35-foot high facades that are separated by 30 feet.) Transition Overlay-2 has 95 horizontal feet between two, 35 foot high facades with a street arterial and traffic between.

4. Creation of a TC-5 Zone – During the May 5 hearing, the Commission heard a proposal to create a TC-5 zone for the area around Firlands Way that would have a 35-foot height limit throughout. The rationale for proposing this height limit is that it would respect the historic heights of buildings that used to exist in the area and improve views of area trees. In discussion of this idea, a Commissioner remarked that it was their belief that greater building height would diminish the pedestrian activity at the street level.

Staff recommends not creating a TC-5 zone because:

- a) *It is staff's understanding that pedestrian activity is mostly unaffected by building heights if the design standards on the ground level are conducive to pedestrian activity. Additional building height brings in a resident and working population that helps business thrive and increases the pedestrian activity day and night*
- b) *There are neither existing historic buildings nor provided documentation of historic buildings by which to gauge future heights.*

5. Firlands Way Policies – At the May 5 hearing, Commission suggested the concept of developing incentives to aggregate properties along Firlands Way to improve development potential. Staff agrees with the concept that it would be helpful for a developer to aggregate properties, because most of the properties along Firlands Way are relatively small and therefore will be more difficult to redevelop. However, the only incentive that staff could devise would be one that would reduce the Town Center building height in this area and then give it back if properties are aggregated. *Staff does not recommend this approach given that the current zoning of the area of MUZ and Office and their respective heights of 50 feet and 65 feet. Staff believes that such an approach would deter future development rather than encourage it and that the benefits of site aggregation are already apparent to developers.*

Another idea raised at the public hearing was to inquire as to whether there is a possibility of identifying the Firlands area as an historic district so that future developers would have access to tax credits or other subsidies from state, local, or federal sources. The key is that government funding requires historic structures to exist to be restored. The only purpose of a historic overlay in the development code would be if there were different land use and development regulations to administer to historic structures. *Staff recommends that Policy TC-23 of the Town Center Subarea Plan (Attachment 4) reads as an alternative approach to promoting Shoreline's history is as follows: "Celebrate the heritage of the community through preservation, education and interpretation of artifacts and places in or near Town Center. Work with the Shoreline Historical Museum to explore the possibilities for a "Town Center Heritage Walk" that includes, but not limited to, Firlands Way, Ronald School, Masonic Temple, the Red Brick Road, and historic events to help activate the Park at Town Center.*

6. Industrial Uses – A Commissioner asked if the prohibition on industrial uses in the Town Center code would prohibit the assembly of non-polluting, manufacturing uses such as software development or small computer hardware assembly. The City uses the NAICS manual of land use classifications. The NAICS uses the term "manufacturing" instead of "industrial" uses. Manufacturing can be the range of heavy, polluting types to the light, non-polluting types. *Staff recommendation is to amend the land use chart to either specify the types of exempt manufacturing such as software and computer assembly or be more general and use the exemption for light manufacturing that is non-polluting and requires no outdoor storage.*
7. Significant Amendments to the proposed Development Code - There are many small, clean-up edits and the larger, topical amendments (listed below) are shown in legislative markup in Attachment 5.

- 20.92.020(A) - Shipping Containers
- 20.92.040.E - Traffic impact studies
- 20.92.060.B.3 - Building façade window area on Boulevard Streets
- 20.92.060.C - Corner building treatment
- 20.92.060.E - Parking standards and incentives
- 20.92.060.I.2 - Mechanical equipment screening
- 20.92.070.B.4 - Maximum façade
- 20.92.070.B.5 - Window trim color.
- 20.30.297 - Design review criteria

B. Public Comment to date

Three public comments have been logged into the public hearing record (see List of Exhibits). There has been no additional comment received on the proposed Subarea Plan or Development Code since the May 5 Public Hearing. One comment was received regarding the Draft SEIS, entered into the record as Exhibit 14 (Attachment 6).

All public comments regarding Town Center, since the beginning of the project in 2007, are available on the City's website on the Town Center project page:
<http://shorelinewa.gov/index.aspx?page=180>.

IV. NOTICE AND SCHEDULE

A 15 day notice was placed in the newspaper for the June 2 public hearing (Attachment 2).

The DSEIS has been available for public review since May 10, 2011 and has been provided to the Planning Commission. The DSEIS comment period will end on June 9, 2011.

Staff requests that the Commission continue the June 2 hearing to June 16, and plan to deliberate and forward its recommendations at the conclusion of the June 16 meeting. Staff will provide all comments on the DSEIS for your record before you close the hearing.

Staff is currently scheduled to review your recommendations with the City Council on July 5 and July 25. We hope that the Council will be able to adopt the Town Center Subarea Plan and Development Code on July 25th.

If you have questions or comments on the staff report, please contact Paul Cohen, Project Manager, at pcohen@shorelinewa.gov or 206-801-2551.

V. ATTACHMENTS

- Attachment 1 - Updated List of Exhibits
- Attachment 2 - Notice of June 2 Public Hearing (Exhibit 9)
- Attachment 3 - Transition Overlay Comparisons (Exhibit 11)
- Attachment 4 - Proposed Town Center Subarea Plan, 4/29/11 (Exhibit 3)
- Attachment 5 - Updated Town Center Development Code, 5/25/11 (Exhibit 12)

Attachment 6 - Comment on DSEIS, Michael Derrick, Ronald Wastewater (Exhibit 14)
Attachment 7 - Draft Supplemental Environmental Impact Statement available online
(Exhibit 13)

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PUBLIC HEARING RECORD
Town Center Subarea Plan and Zoning
May 5, 2011 / List of Exhibits

- Exhibit 1** May 5, 2011 Staff Report “Public Hearing on Town Center Subarea Plan and Development Code”
- Exhibit 2** Notice of May 5, 2011 Public Hearing
- Exhibit 3** Proposed Town Center Subarea Plan, dated 4/29/11
- Exhibit 4** Proposed Town Center Development Code, dated 3/31/11
- Exhibit 5** SEPA Checklist
- Exhibit 6** Letter from Boni Biery, dated 4/22/11
- Exhibit 7** Letter from Janet Way, President, Shoreline Preservation Society, dated 5/5/11
- Exhibit 8** Written testimony from Vicki Westberg, submitted at 5/5/11 Public Hearing

June 2, 2011 / List of Exhibits

- Exhibit 9** Notice of June 2, 2011 Public Hearing
- Exhibit 10** June 2, 2011 Staff Report “Second Public Hearing on Town Center Subarea Plan and Development Code”
- Exhibit 11** Transition overlay comparisons and proposed TC-5

- Exhibit 12** Proposed Town Center Development Code, dated 5/25/11
- Exhibit 13** Town Center Subarea Planned Action - Draft Supplemental Environmental Impact Statement, May 2011
- Exhibit 14** Email comment regarding Draft Town Center Subarea Planned Action SEIS from Michael U. Derrick, Ronald Wastewater District, sent 5/10/11

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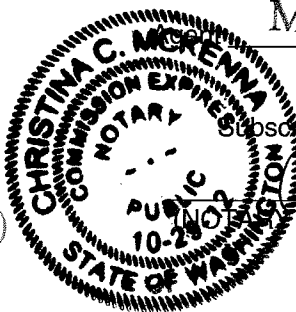
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STATE OF WASHINGTON
Counties of King and Snohomish

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Subscribed and sworn to before me on May 12, 2011
(DATE)

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(SIGNATURE) Notary Public in and for the State of Washington, residing at Seattle

Christina C. McKenna



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Ad TEXT: City of Shoreline Notice of 2nd Public Hearing of the Planning Commission for the Town Center Subarea Plan and Development Code Amendments

The City is proposing the creation of a Town Center plan that is located between N 170th and N 188th Streets and between Fremont Ave N and Stone Ave N. The plan is to guide development into a distinctive district that encourages public services, retail services, improved design standards, residences, a park, adjoining single-family protections, and a walkable environment that is serviced by Bus Rapid Transit.

Interested persons are encouraged to provide oral and/or written comments regarding the above project at an open record public hearing. The hearing is scheduled for June 2, 2011 at 7 pm in the City Council Chambers, City Hall at 17500 Midvale Ave N, Shoreline, WA.

The Draft SEIS has been noticed and written comments will be received until June 9, 2011 at 5 pm. Public comments on the Draft SEIS will also be accepted during the June 2 Planning Commission public hearing.

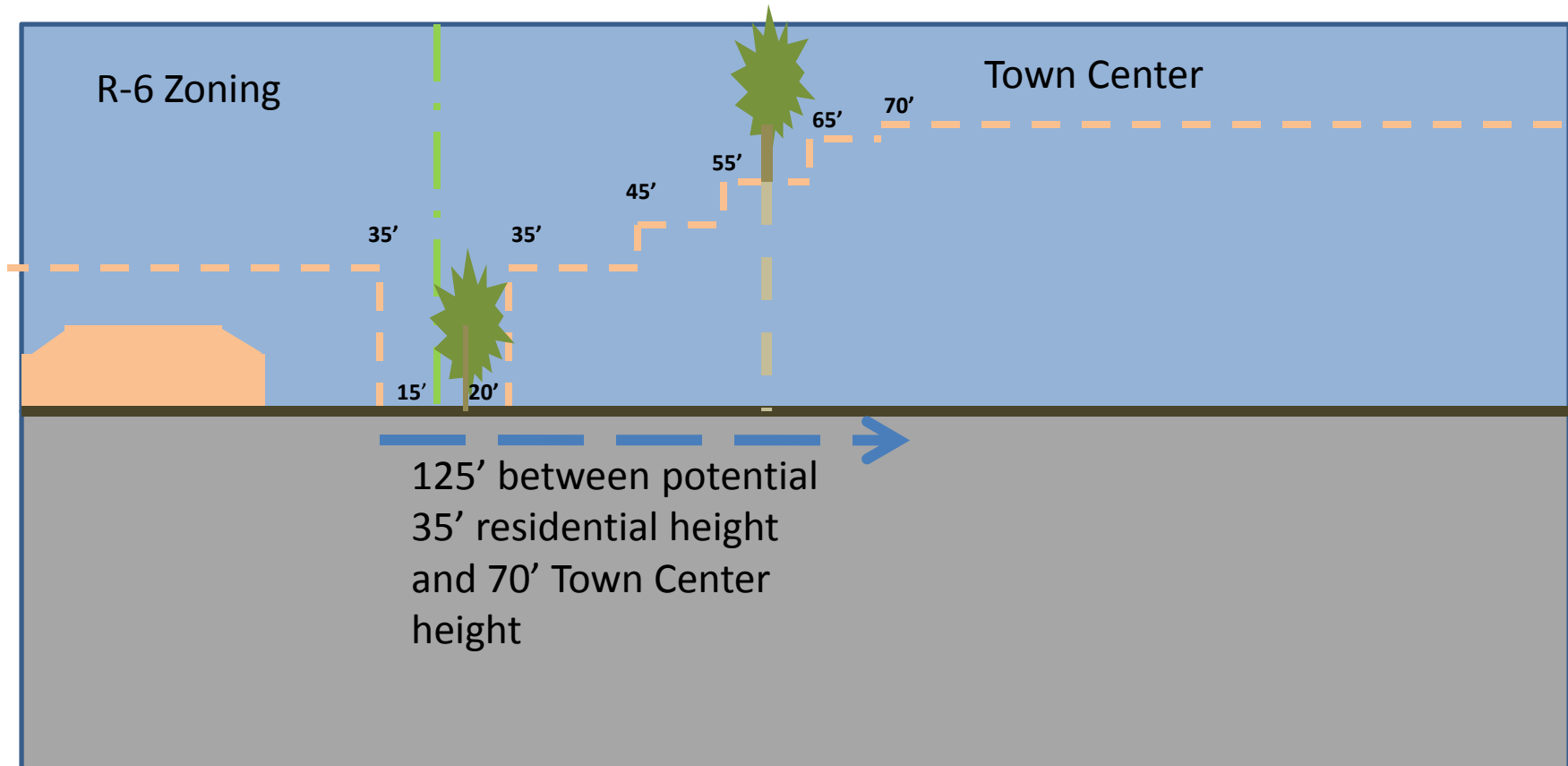
Copies of the proposed subarea plan and development code amendments, and Draft SEIS are available for review at the City Hall, 17500 Midvale Avenue North.

Questions or More Information:
Please contact
Paul Cohen, Planning and
Development Services at
(206) 801-2551.

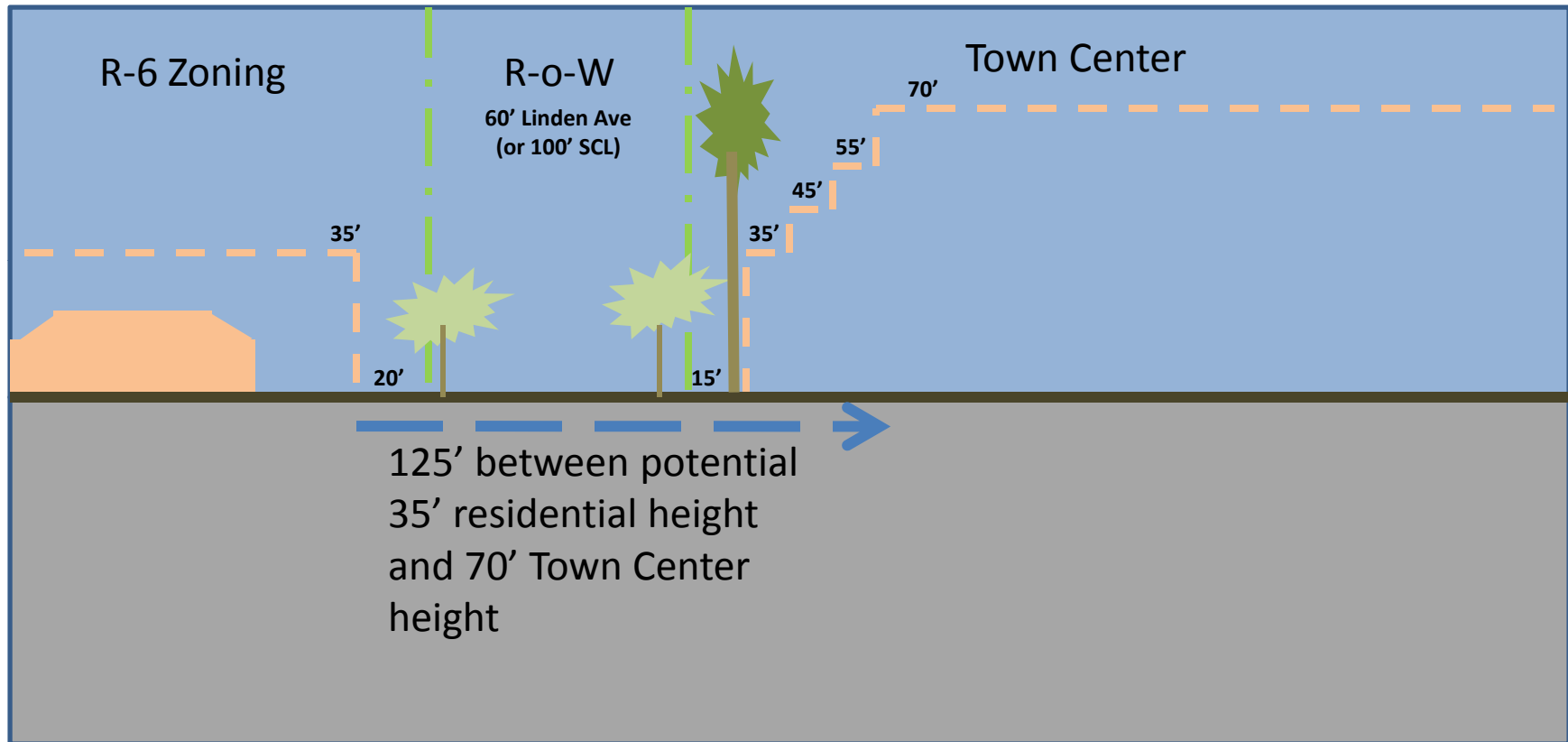
Any person requiring a

disability accommodation should contact the City Clerk at (206) 801-2230 in advance for more information. For TTY telephone service call (206) 546-0457. Each request will be considered individually according to the type of request, the availability of resources, and the financial ability of the City to provide the requested services or equipment.

Transition Overlay 1



Transition Overlay 2



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DRAFT 4/29/11



Shoreline Town Center Subarea Plan

Introduction

Located on the middle mile of the City's three mile long Aurora corridor (State Route 99), Town Center is the geographic center of the City of Shoreline. It is at the crossroads of three of the City's most heavily traveled roads, N. 175th St, N. 185th St., and Aurora/SR 99, and serves as the civic and symbolic center of the community. See Fig. 1. Early in the life of the new City of Shoreline, a citizen survey identified this area as the "Heart of Shoreline."

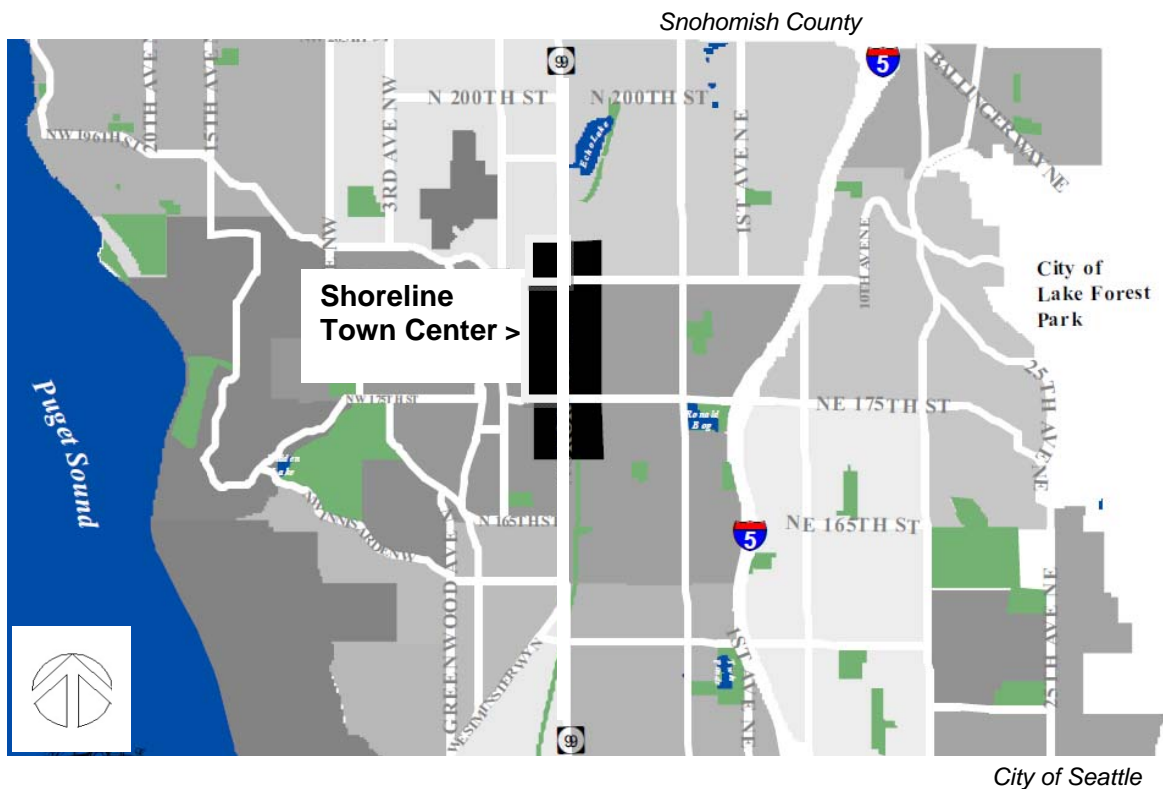


Fig.1 *Town Center is the Heart of Shoreline*

Shoreline's settlement began in the early 20th century in this area around Judge Ronald's original homestead and the Ronald schoolhouse. In the early 1900's, the North Trunk (red brick) Road and Interurban electric railway traversed this area, linking it to Seattle and Everett. The "Ronald Station" was located in the vicinity of the proposed Park at Town Center.

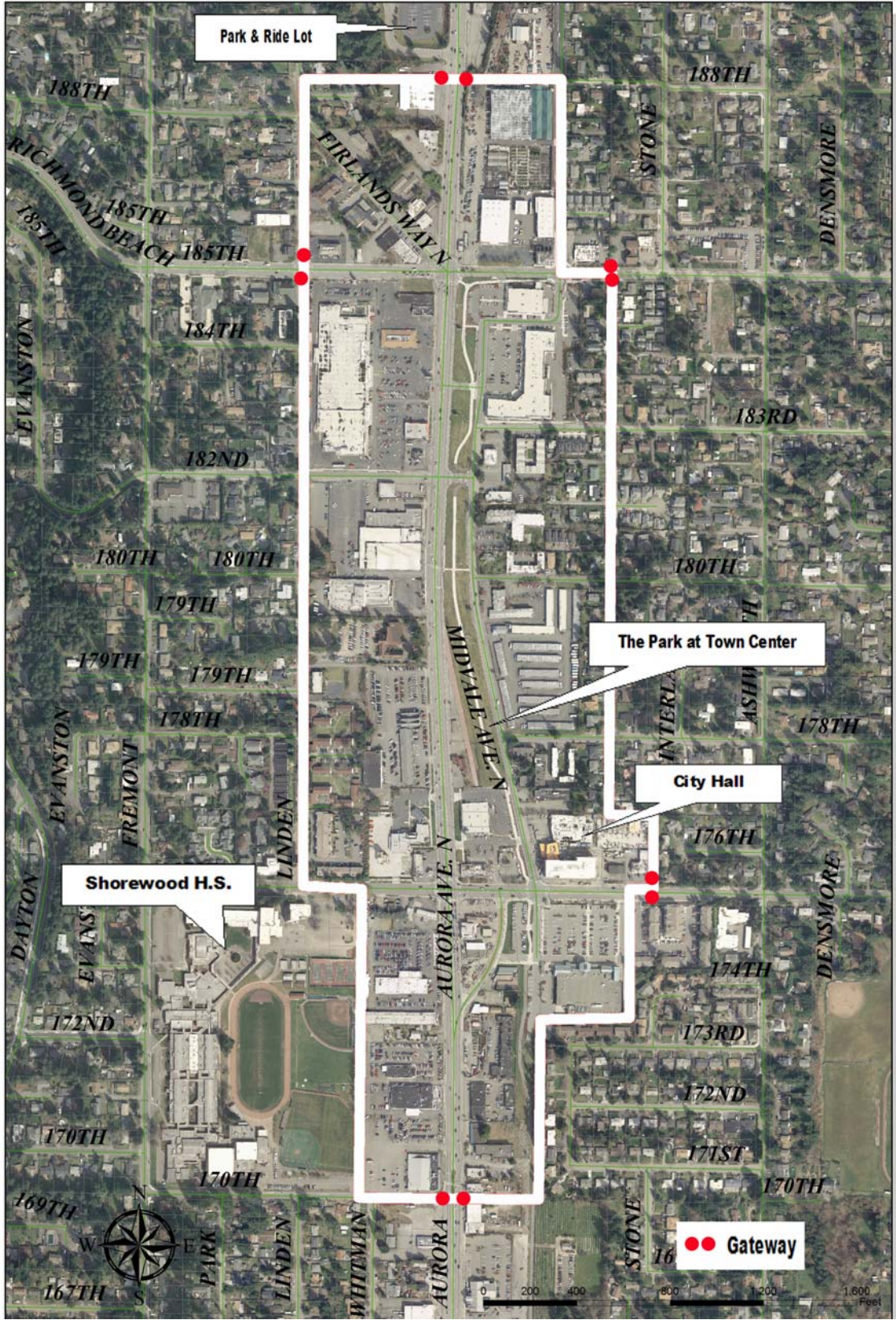


Fig.2 Town Center boundaries and gateways

Growing dramatically after World War II, Shoreline became an auto-oriented suburb characterized by large areas of relatively low residential density, which lacked urban amenities and services such as parks and sidewalks. During the post-war decades, the Aurora/SR 99 corridor developed as a strip commercial highway, with a tremendous diversity of businesses. While these businesses largely met local and regional needs, the highway itself became congested, chaotic, unattractive and unsafe.

Several of the civic facilities typically found in traditional downtowns began to locate in and around the Town Center area in the 1960's. These include the Shorewood High School, the Shoreline Fire Department Headquarters, and the Ronald Sewer District Office and Yard. Commercial and apartment uses also began to locate in this area, including grocery, drug store and other retail stores and personal services. These still co-exist with businesses serving a larger market area, such as auto dealerships.

The emergence of regional shopping malls at Alderwood and Northgate in the 1970's began to erode Shoreline's primary market for certain retail goods and services. With the City's incorporation in 1995, additional civic pieces of an emerging Town Center came into being. The Interurban Trail through Town Center was completed in 2005 and the new City Hall opened in 2009. In 2011 Aurora Avenue North through Town Center was rebuilt as a Boulevard, design work began on a new park at Town Center and construction began on a new Shorewood High School, with buildings located immediately adjacent to the Town Center.

In 2009, the City adopted a city-wide Vision Statement which articulated the community's preferred future for the year 2030. The Vision integrated many of the policy objectives of the City's adopted strategies for Economic Development, Housing, and Environmental Sustainability. The Vision identifies Town Center as a focal point for much of the City's future growth accommodation, and many of the framework goals provide a broad outline for much of the content of the Town Center Subarea Plan.

Achieving the City's Vision and the objectives of the Town Center Subarea Plan will be influenced by regional market factors, individual investment decisions, and state and regional growth management policies. High capacity transit service will arrive on Aurora by 2013 in the form of bus rapid transit service, while regional light rail service is scheduled for 2023, linking Shoreline to the broader region.

The growth management development strategy for the Central Puget Sound region, Vision 2040, forecasts adding 1.7 million people and 1.4 million jobs with only a negligible increase in the size of the region's urban growth area. See Fig. 3. Combined with state climate change targets to reduce greenhouse gas emissions and vehicle miles traveled, there will be strong market and regional public policy pressures on close-in cities such as Shoreline to accommodate growth.

Shoreline's ability to accommodate these pressures while maintaining the community's reputation as one of America's best places to live, will be a major challenge. Implementation of a clearly articulated Town Center Subarea Plan will be one important strategy to help Shoreline meet that challenge.



Fig. 3 Shoreline's place within the Vision 2040 Urban Growth Area

Town Center Vision Statement

Shoreline Town Center in 2030 is the vibrant cultural and civic heart of the City with a rich mix of housing and shopping options, thriving businesses, and public spaces for gatherings and events. People of diverse cultures, ages, and incomes enjoy living, working, and interacting in this safe, healthy, and walkable urban place.

Once a crossroads on the Interurban electric railway that connected Seattle and Everett, Shoreline's Town Center has evolved into a signature part of the City. The Center stands out as a unique and inviting regional destination while gracefully fitting in with its surrounding landscape and neighborhoods. Connections to neighborhoods and the region are convenient and accessible through a system of paths, roads, and public transit. Citizens, business owners, and city officials are justifiably proud of the many years of effort to create a special and livable place that exemplifies the best of Shoreline past, present, and future.

Town Center is anchored along N. 175th St. by the City Hall complex, Shorewood High School, the Shoreline Fire Department Headquarters, and the Ronald Sewer Offices and Yard. The linear park at Town Center between Aurora Boulevard and Midvale Avenue North provides a green thread through the center of the area. City Hall serves not only is the seat of government, but also provides an active venue for many other civic functions. The north end of Town Center includes the revitalized historic five-point interchange at Firlands.

Town Center is a physically and visually attractive, inviting, and interesting place where form and function come together to promote a thriving environment for residents, businesses, and visitors. Notable features include a number of green open spaces both large and intimate, enclosed plazas, storefronts opening onto parks and wide sidewalks, underground and rear parking, numerous ground-floor and corner retail options within mixed-use buildings, and internal streets within large blocks and other pathways that provide safe, walkable connections throughout the Center area east, west, north, and south.

Building heights range from one to three stories within transition areas adjacent to single-family residential areas along Linden and Stone avenues, up to six stories in mixed-use buildings along sections of Aurora Boulevard, while buildings in the Midvale and Firlands areas are generally four to five-story mixed-use structures. Building materials, facades, designs, landscaped setbacks, as well as public art and green infrastructure features represent a wide variety of styles and functions while maintaining a harmonious look and feel.

The City of Shoreline has long been committed to the realization of the three E's of sustainability -- environmental quality, economic vitality and social equity -- and Town Center has successfully integrated these values to achieve sustainable development.



Fig. 4 *Principles of Sustainable Development*

Environmental Quality

While respecting elements of its historic character, Town Center has become a model of environmentally sound building and development practices. The buildings themselves are state-of-the-art energy efficient and sustainable structures with zero carbon impacts. Town Center's tree canopy and native vegetation are all part of a strategic system for capturing and treating stormwater on site and protecting and enhancing overall environmental quality. Major transit stops along the mature Aurora Boulevard provide quick and convenient connections to major centers elsewhere in the region. Civic spaces and parks have been designed for daily use and special events.

Economic Vitality

Town Center attracts a robust mix of office, service, and retail development. The boulevard boasts an exciting choice of shops, restaurants, entertainment, and nightlife. The Center is a model of green industry and economic sustainability that generates the financial resources that help support excellent city services, with the highest health and living standards. As a result, Town Center's success helps to make Shoreline one of the most fiscally sound and efficiently run cities on the West Coast.

Social Equity:

Town Center offers a broad range of job opportunities and housing choices that attract a diversity of household types, ages, and incomes. Attention to design allows the public gathering places to be accessible to all. People feel safe here day and night. Festivals, exhibits, and performances attract people of all ages and cultural backgrounds.

Summary:

Town Center is thoughtfully planned and built, yet all the choices feel organic and natural, as if each feature and building is meant to be here. Town Center is a place people want to be in Shoreline in 2030, and is positioned to continue to grow gracefully and sustainably for decades.

Town Center Goals

Goal TC-1 Create a Town Center that embodies the sustainability values of environmental quality, economic vitality and social equity.

Goal TC-2 Create a Town Center that is complete, compact, and connected to its neighborhoods and the region.

Goal TC-3 Create a "sense of place" in Town Center that provides a focal point for Shoreline's civic life and community-wide identity.

Goal TC-4 Create an economically and culturally thriving Town Center through the coordinated efforts of the City, the School District, business organizations, community non-profits, and neighborhood associations.

Town Center Policies

Policy TC-1 Create a safe, attractive, and walkable Town Center that links mixed use, mid-rise buildings, a broad range of housing choices, major civic amenities, public gathering places, and bus rapid transit service.



Fig.5 *Mid-rise, mixed use buildings provide pedestrian scale and access at street level*

Policy TC-2 Publicize innovative “green infrastructure” including City Hall, Shorewood High School, and Aurora boulevard as models for private projects in Town Center.



LOW IMPACT IMAGE
BUT NOT SILVA CELL

Fig. 6 *The LEED GOLD City Hall, LEED SILVER Shorewood High School, and low-impact drainage facilities in the Aurora project set a high bar for sustainability in new projects*

Policy TC-3 Promote a blend of civic, commercial, and residential uses in Town Center.

Policy TC-4 Increase the variety of housing choices in Town Center and increase opportunities for moderate cost housing. Reduce new housing construction costs and incentivize affordable housing in Town Center by reducing parking requirements and pursuing an aggressive program of Property Tax Exemptions.

Policy TC-5 Encourage additional retail, service, grocery, and restaurant uses to serve people who live or work in Town Center or within walking distance of it.

Policy TC-6 Leverage federal, state and other investments and market Town Center as a high value location for private investment and business starts.



Fig. 7 *The Interurban Trail, Aurora Project and Bus Rapid Transit service*

Policy TC-7 Give clear visual indication of Town Center's boundaries with gateway treatments, such as signs and landscaping.



Fig. 8 *Examples of town center entry signs*

Policy TC-8 Create a hierarchy of Boulevard, Storefront, and Greenlink streets to serve different mobility and access roles within Town Center.



Fig. 9 *A hierarchy of boulevards, storefront streets and greenlink streets*

Policy TC-9 Post public “wayfinding” signs to direct motorists and bicyclists to public destinations within and near Town Center.



Fig 10. Wayfinding signs can be located in medians, behind sidewalks, or on poles

Policy TC-10 Create a seamless network of safe, convenient, and attractive walkway improvements within Town Center that also connects to all streets, the Interurban Trail, high capacity transit on Aurora, and adjacent neighborhoods.

Policy TC-11 Connect Town Center to other parts of Shoreline and the region by promoting multi-modal transportation choices including high capacity transit on Aurora, frequent local bus service, bicycle paths, and improved pedestrian walkways.

Policy TC-12 Create safe and attractive pedestrian crossings of Aurora, walkways to better link uses within Town Center, and more direct and attractive walkways from adjacent neighborhoods.

Policy TC-13 Reduce the noise, visual, and safety impacts of traffic on Aurora Avenue as it passes through the Town Center.

Policy TC- 14 Encourage the removal of the western leg of the intersection at N. 182th and Aurora if re-development of lands at N. 180th and Aurora enables the installation of a fully signalized mid-block intersection at that location.

Policy TC-15 Consider the creation of new rights of way or the vacation of other rights of way in order to facilitate better vehicular and pedestrian circulation. Encourage parcel aggregation and more comprehensive site development designs in order to create a more pedestrian friendly environment and promote mixed use development.

Policy TC-16 Protect adjacent residential areas from impacts generated by developments in Town Center. Create a medium density buffer between the commercial uses in Town Center and the single family neighborhoods east of Midvale and limit lighting, signage, and noise impacts. Orient commercial uses west of Aurora so that they have primary access and impacts oriented toward Aurora, rather than to the neighborhood west of Linden.

Policy TC-17 Reconfigure Midvale Avenue N. between N. 175th St. and N. 182nd St. as a low speed, pedestrian-friendly lane with back-in angle parking to support mixed use development on the east side and public uses in the Town Center Park.



Fig. 11 *Midvale Ave N. concept with landscaping, crosswalks, and back-in angle parking*

Policy TC-18 Recognize the environmental and aesthetic value of existing stands of prominent trees, promote a green built environment by adopting the U.S. Green Building Code, and launch a recognition program for innovative private projects that exemplify the sustainability vision for Town Center.

Policy TC-19 Develop the park at Town Center as a memorable, green, open space and link it to the City Hall Civic Center. Program both of these spaces for celebrations, public gatherings, and informal “third places.”



Fig. 12 *Farmers' markets, parades, lawn sports, and wi-fi access are several possible park uses*

Policy TC-20 Enhance the sustainability of adjacent residential neighborhoods through targeted investments in green street links to Town Center, and focused programs to enhance energy conservation and carbon neutrality.

Policy TC-21 Encourage structured parking for commercial, multifamily, and mixed use developments, and reduce parking requirements in recognition of the availability of transit, on-street parking, walkability, and housing types.

Policy TC-22 Where feasible, minimize surface parking lots and locate them in rear or side yards and screen them with landscaping, low walls or fences, arbors, and other treatments to soften visual impacts.

Policy TC-23 Celebrate the heritage of the community through preservation, education and interpretation of artifacts and places in or near Town Center. Work with the Shoreline Historical Museum to explore the possibilities for a “Town Center Heritage Walk” and programs to help activate the Park at Town Center.



Fig. 13 *Town Center history: the Red Brick Road, Ronald School House, Interurban Station*

Policy TC-24 Abate the remaining billboards, or re-locate them out of the Town Center, and craft a form-based sign code that orients and sizes commercial signage based on the function and speed of serving streets and walkways.

Policy TC-25 Create a form-based development code and streamlined permit process that consolidates environmental review and design review into a single expedited administrative permit review. Adopt illustrated and clear design standards with a menu of options and opportunities for design flexibility.

Policy TC-26 Adopt Town Center design standards and a design review process so that new projects respect existing architectural patterns (e.g., building forms, roof shapes, fenestration, materials, etc.) that provide context and human scale.



Fig. 14 *Town Center roof shapes of various pitches, materials, colors*

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5/25/11

Town Center District Zone

- 20.92.010 Purpose
- 20.92.020 Zoning, Land Use, and Form
- 20.92.030 Street Types and Pedestrian Circulation Map
- 20.92.040 Neighborhood Protection Standards
- 20.92.050 Street Frontage Design Standards
- 20.92.060 Site Design Standards
- 20.92.070 Building Design Standards
- 20.92.080 Sign Design Standards

- 20.30.297 Design Review Approval
- 20.50.021 MUZ Design Review Amendments
- 20.91.040 Ridgecrest Design Review Amendments
- 20.20 Definitions

20.92.010 Purpose.

A. Establish standards for the Town Center ~~District Zone~~. These standards implement the policies of City of Shoreline Comprehensive Plan and Town Center Subarea Plan through code requirements for use, form, design, and process.

Comment [p1]: District is the entire town center and zones TC-1 thru 4

~~B. Some standards within this chapter apply only to specific types of development and zones as noted. Standards that are not addressed in this chapter will be supplemented by the development standards in Chapter 20.50 SMC. In the event of a conflict between standards, the standards of this chapter shall prevail.~~

Comment [p2]: Moved to 20.92.020.3

~~C.B.~~ Set forth a procedure designating certain land use actions within the boundaries of the geographic area described in the Town Center ~~Zone~~ Subarea Plan as Town Center ~~District Zone~~ as "planned actions" consistent with RCW 43.21.031, WAC 197-11-164 to 197-11-172, and SMC 20.30.640.

~~D.C.~~ Planned action projects that are within the scope of the planned action EIS determination shall not require a SEPA threshold determination and shall be reviewed as ministerial decisions by applying the provisions of the Development Code. Proposed projects that are not within the scope of the planned action EIS shall require environmental review under SEPA

~~E.D.~~ Design Review Approval under SMC 20.30.297 is required for all development proposals prior to approval of any construction permit. A permit applicant wishing to modify any of the standards in this chapter may apply for a design departure under SMC 20.30.297.

20.92.015 Threshold – Required for site improvements.

The purpose of this section is to determine how and when the provisions for site improvements cited in the Town Center District development standards apply to development proposals. Full site improvements are required if the development is:

- ~~Ce~~completely new development; or
- The construction valuation exceeds 50 percent of the existing site and building valuation.

20.92.020 Zones, Land Use, and Form.**A. Town Center District Zones**

In order to implement the vision of the Comprehensive Plan's Town Center Subarea Plan, there are Town Center (TC) zones established as shown in Figure 20.92.030.

1. Four zones are delineated within the Town Center that has general and specific design standards.
 - a. TC-1: This zone allows for a broad range of uses similar to TC-2 with the exception to allow vehicle sales, leasing, and servicing.
 - b. TC-2: This zone includes property fronting on Aurora Avenue, N. 175th and N. 185th streets and provides the widest range of uses and development potential with pedestrian activity primarily internal to the sites.
 - c. TC-3: This zone is oriented toward smaller arterials with a wide range of uses that focus pedestrian activity primarily along street frontages.
 - d. TC-4: This zone is oriented around Stone Avenue and limits the residential heights, uses and vehicle circulation to protect the adjacent single family neighborhoods.

2. Transition Overlay: This overlay provides a 120-foot wide transition from higher intensity development to lower intensity uses and protects adjoining single family neighborhoods from large building heights, traffic, and inappropriate land uses.

3. Some standards within this chapter apply only to specific types of development and zones as noted. Standards that are not addressed in this chapter will be supplemented by the development standards in Chapter 20.50 SMC. In the event of a conflict between standards, the standards of this chapter shall prevail.

2.4. _____

Comment [p3]: Needs dimension because the zoning map is not specific enough.

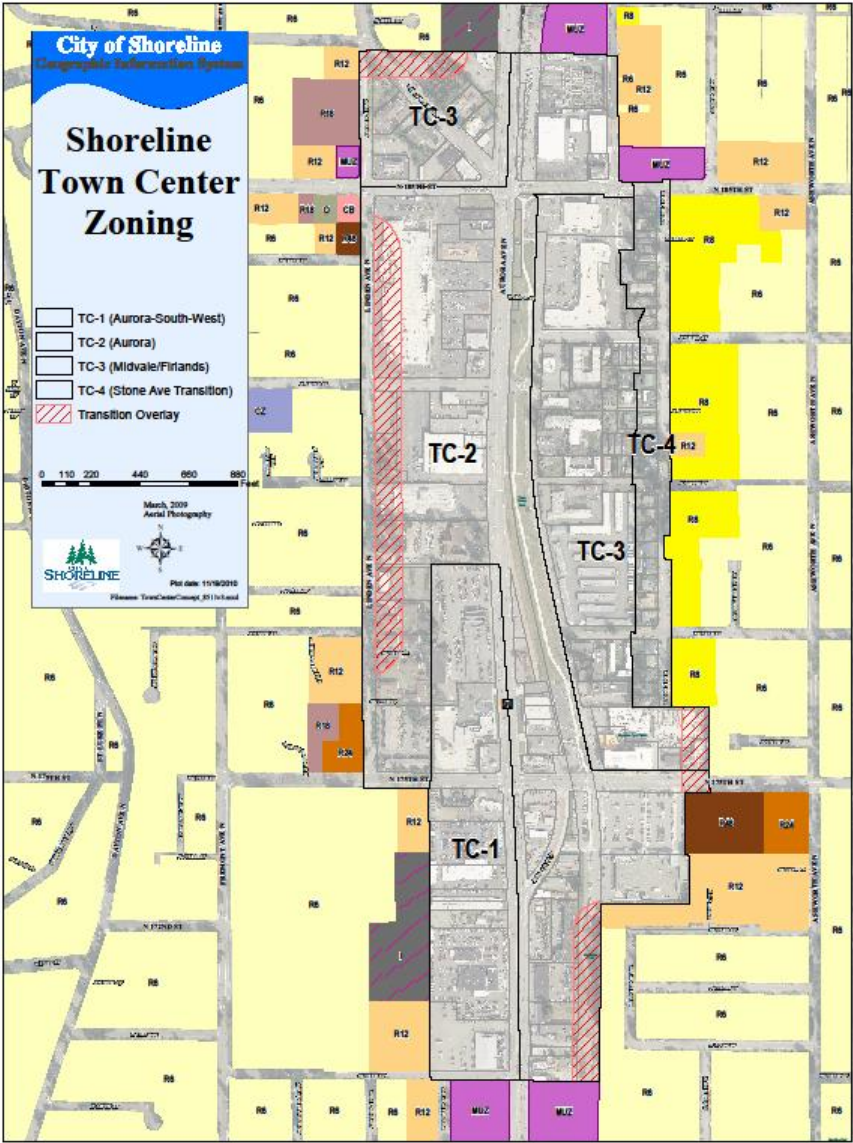


Figure 1. 20.92.020

- B. Table 20.92.020(A) lists general categories of permitted land uses for each of the Town Center zones. The general categories for permitted uses include all of the specific uses listed in the corresponding tables cited, except for those listed in this table as “prohibited uses.” If further clarification is required, the Director shall issue an administrative determination consistent with the provisions of this Chapter and the policy guidance of the Town Center Subarea Plan.

Table 20.92.020(A) Land Use Chart

General Land Use Category	Specific uses listed in Table	TC-1 Aurora SW	TC-2 Aurora	TC-3 Midvale /Firlands	TC-4 Stone Ave Resid.
Detached Single Family	20.40.120				
Duplex, Apt, Single Family Attached	20.40.120				
Group Residences	20.40.120				
Lodging	20.40.120	PERMITTED USES			
Health Facility	20.40.140				
Government Facility	20.40.140				
Automotive fueling and service Stations	20.40.130				
Retail, Eating, and Drinking	20.40.130				
Personal and Business Services	20.40.130				
Vehicle Sales, Leasing, and Service(2)	20.40.130				
<u>Shipping Containers</u>					
Gambling Uses					
Wrecking Yards		PROHIBITED USES			
<u>Manufacturing Industrial Uses (Except non-polluting without outdoor storage)</u>					
Adult Use Facility					

Comment [p4]: New Land Use

Comment [p5]: Refined to allow light manufacturing that is non polluting without outside storage.

Table 20.92.020(A)

Table 20.92.020(B) Form.

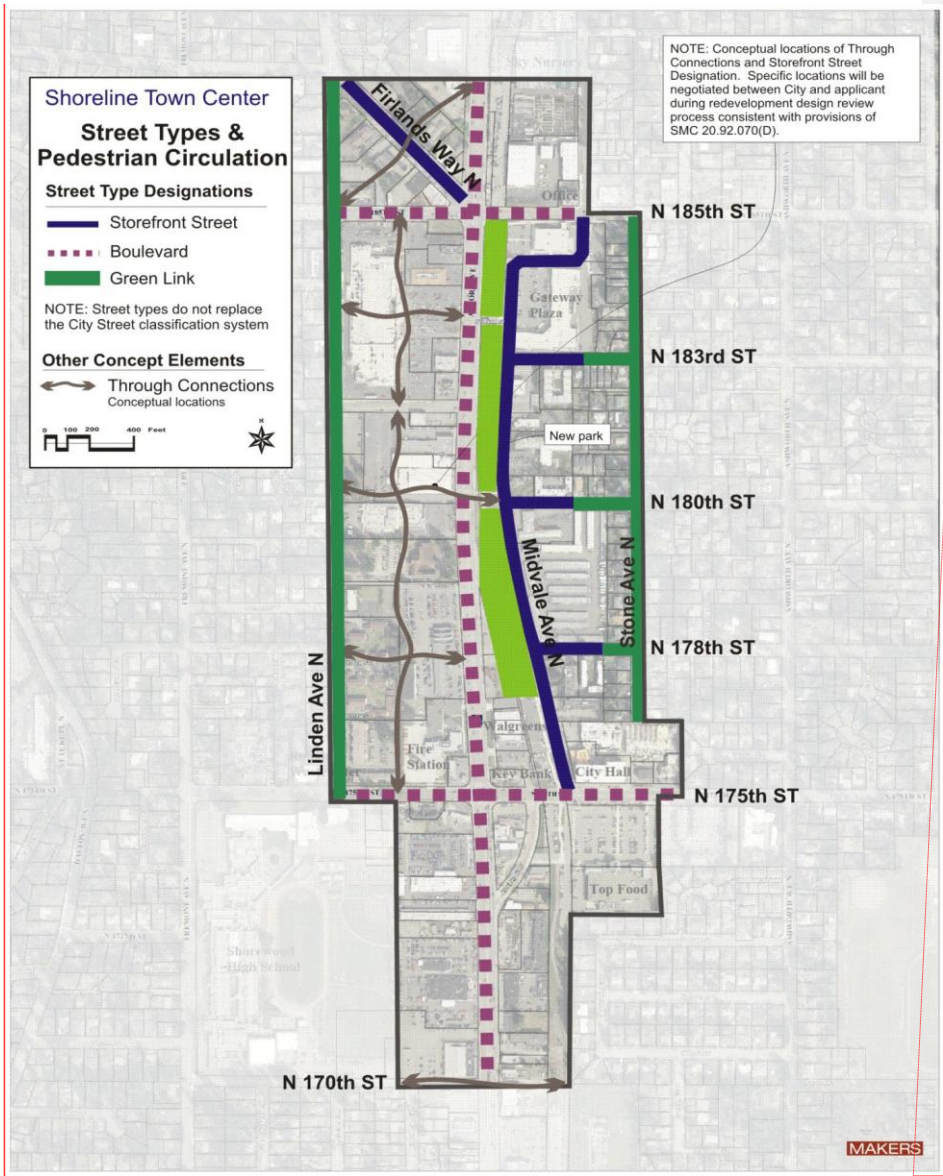
	TC-1 Aurora SW	TC-2 Aurora	TC-3 Midvale/ Firlands	TC-4 Stone Ave Res	Transition Overlay
Minimum Front Yard Setback (1)(2)(3)	0-10 ft (6)	0-10 ft	0-10 ft	15 ft	15 ft
Minimum Side Yard Setback from Nonresidential Zones (4)	0 ft	0 ft	0 ft	5 ft (5)	5 ft (5)
Minimum Rear Yard Setback from Nonresidential Zones	0 ft	0 ft	0 ft	5 ft	0 ft
Minimum Side & Rear Yard (Interior) Setback from R-4 & R-6	15 ft	15 ft	15 ft	5ft	20ft
Minimum Side & Rear Yard Set- back from R-8 through R-48 and TC-4	15 ft	15 ft	15 ft	5 ft	15 ft
Maximum Height (5)	70 ft	70 ft	70 ft	35 ft	35 ft
Maximum Hardscape Area	95%	95%	95%	75%	75%

Exceptions to Table 20.92.040(A).

- (1) *Unenclosed porches and covered entry features may project into the front yard setback by up to 6 feet. Balconies may project into the front yard setback by up to 2 feet.*
- (2) *Additional building setbacks may be required to provide right-of-way and utility improvements.*
- (3) *Front yard setbacks are based on the applicable street designation. See figure 20.92.020 for the street designation and SMC 20.92.070(B) for applicable front yard setback provisions.*
- (4) *These may be modified to allow zero lot line developments for internal lot lines only.*
- (5) *See section 20.92.050.C for height step-back standards.*
- (6) *Front yards may be used for outdoor display of vehicles to be sold or leased in the TC-1 zone.*

20.92.030 Street Types and Pedestrian Circulation.

This map illustrates site-specific design elements to be implemented by code for street types and Through Connections.



Comment [p6]: Added color for Greenlink streets

20.92.040 Neighborhood Protection Standards.**A. Purpose**

- Minimize negative impacts of Town Center development on adjacent single family neighborhoods.
- Enhance residential neighborhoods on both sides of Linden and Stone Avenue North.

B. Applicability

Unless specifically noted, the standards herein apply to properties within zone TC-4 and the Transition Overlay identified in the Town Center Zoning Map in figure 20.92.030 and other Town Center properties that are directly adjacent to those zones.

C. Building Heights

The maximum building height is 35 feet for the first 50 horizontal feet perpendicular from ~~the front abutting~~ property lines, face across streets or radii from corners of R-4 or R-6 zones. For each subsequent 20 feet from the property line an additional 10 feet in height is allowed up to the maximum height of the underlying zoning.

Comment [p7]: May be modify if 2 transition overlay types are used.

D. Site Access

Direct commercial vehicular and service access to a parcel shall not be from Stone or Linden Avenues unless no other access is available or practical as determined by the City.

E. Traffic Impacts

All development in the Town Center shall conduct a traffic impact study per its guidelines. Any additional traffic that is projected to use non-arterial streets shall and implement traffic mitigation measures which are approved by the city's traffic engineer in collaboration with the abutting neighborhoods that are directly impacted, to mitigate potential cut-through traffic or parking impacts to single family neighborhoods.

Comment [p8]: Specific when a study is required

Comment [p9]: Collaboration with the neighbors is key to finding a satisfying solution.

F. Setbacks and Buffers

Buildings in zones TC-2 and TC-3 shall have a ~~20~~15-foot wide, Type I landscape with an 8-foot solid fence or wall adjacent to zone TC-4 and R-6 parcels in addition to any required open space.

G. Tree Preservation

20 percent of all healthy, significant trees for each parcel must be preserved in TC-4 and Transition Overlays portions of private property per SMC 20.50.290.

20.92.050 Street Frontage Design Standards.**A. Purpose**

- Enhance the appeal of street frontages to encourage people to walk and gather.
- Establish frontage standards for different streets to:
 - Reinforce site and building design standards in each zone.
 - Provide safe and direct pedestrian access within the Town Center and from adjacent neighborhoods.
 - Minimize conflicts between pedestrians, bicyclists, and vehicular traffic and parking.

B. Applicability

The standards in this section apply only to the sidewalks and the amenity zone in the public rights-of-way. These standards shall meet the City's Engineering ~~Development~~ sign Guide ~~design criteria lines~~. Where there is a conflict, the Director shall determine which applies.

C. Design

1. Storefront, Greenlink, and Boulevard Street frontages, as depicted on Figure 20.92.030, shall have:

- a. A minimum ~~unobstructed sidewalk width of~~ 10 feet for Storefront streets, 8 feet for Green ~~Link~~ streets, and 7 feet for Boulevard streets ~~of unobstructed sidewalk widths~~ and all streets with 5 feet of amenity zone widths;
- b. Storefront, Boulevard, and Greenlink streets shall have street trees spaced on average 30 feet either in tree pits and grates or in amenity ~~zonestrips~~;
- ~~e.~~ Storefront and Green Link streets may have breaks in the amenity ~~zone strip~~ and tree distribution to allow for driveways, ~~sight~~ distance, ~~ADA access~~, utilities, crosswalks, bike racks, ~~on-street parking, and~~ benches, ~~and-or~~ sitting walls. In place of amenity ~~zonestrips~~, ~~street trees pits and in grates~~ ed pits are required ~~parallel to street parking~~;
- ~~d-c.~~ Each development on a Storefront street shall provide a minimum 8 feet of bench or sitting wall;
- ~~e-d.~~ Both sides of Storefront and Green ~~Linklink~~ Streets shall have on-street parking and curb bulb-outs at block ends and pedestrian crossings. On-street parking is optional only if adequate street rights-of-way width do not exist;
- ~~f-e.~~ Utility appurtenances such as signal boxes, hydrants, poles, or other obstructions shall not be placed in the public sidewalk; and
- ~~g-f.~~ When improved, Firlands Way within the Town Center shall expose and restore the brick road bed underneath. If restoration of the brick road is unfeasible or cannot meet City road standards then the City shall design a ~~slow~~ street that allows traffic and pedestrians to mix safely.

1. Rights-of-Way Lighting

- a. One to two-foot candles and ~~between 10-foot and maximum~~ 15-foot in height for sidewalk areas. Lighting shall be located on private property or mounted on building facades.

- b. Street light standards shall be a maximum 25-foot height for street light standards, designed using the Aurora Avenue model and color, modified to meet the 25-foot maximum height, and spaced to meet City illumination standards.



20.92.060 Site Design Standards.**A. Purpose**

- Promote and enhance public walking and gathering with attractive and connected development to:
 - a. Promote distinctive design features at high visibility street corners.
 - b. Provide safe routes for pedestrians ~~and-and disabled~~ people with disabilities across parking lots, to building entries, and between buildings.
- Promote economic development that is consistent with the Town Center Subarea Plan

B. Site Frontage

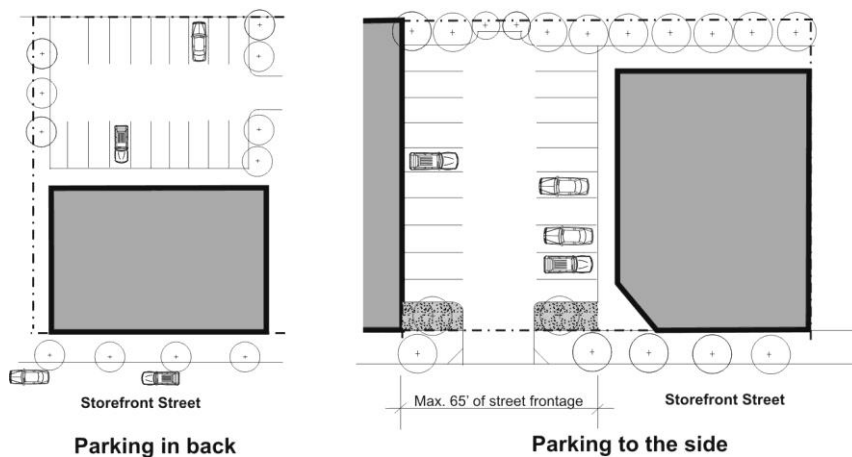
Site design standards for on-site landscaping, walkways, public places, and open space may be combined if their separate minimum dimensions and functions are not compromised. Development abutting street frontages as designated within the Town Center per figure 20.92.030 shall meet the following standards.

1. Storefront Streets

- a. Buildings shall be placed at the property line or at back of planned sidewalk if on private property. However, buildings may be setback ~~f~~urther if Public Places (as specified in SMC 20.92.070(F)) are included or a utility easement is required between the sidewalk and the building;
- b. Minimum transparent window area is 60 percent of the ground floor facade placed between the heights of 30 inches and 8 feet above the ground for each front facade;
- c. The primary building entry shall be located on a street frontage and, if necessary, recessed to prevent door swings over sidewalk or an open entry to an interior plaza or courtyard from which building entries are accessible;
- d. Minimum weather protection at least five feet in depth, along at least 80 percent of the facade width, including building entries; and
- e. Surface parking along Storefront Streets is not more than 65 lineal feet of the site frontage. Parking lots are not allowed at street corners. No parking or vehicle circulation is allowed between the right-of-way and the building front facade. Sites with less than 100 feet lineal feet of frontage are exempt from this standard. See 20.92.070(E)(2)for parking lot landscape standards.



Storefront and Boulevard buildings



Parking lot locations along Storefront streets.

2. Green Link Streets

- a. Minimum front yard setback is 15 feet. Porches and entry covers may project 6 feet into the front yard setbacks;
- b. Transparent window area is 15 percent of the entire façade;
- c. Building entries shall be visible and accessible from a street front sidewalk. An entrance may be located on the building side if visible;
- d. Minimum weather protection is 5-foot deep over building entries;

- e. Landscaped front yards may be sloped or terraced with maximum 3 foot high retaining walls; and
 - f. Surface parking is no more than 65 lineal feet of the site frontage and setback 10 feet from property line. Parking lots are not allowed at street corners. No parking or vehicle circulation is allowed between the right-of-way and the building front facade. See 20.92.060(F)(3) for parking lot landscape standards.
2. Boulevard Streets
- a. Minimum transparent window area is 60 percent of the ground floor facade placed between the heights of 30 inches and 8 feet above the ground for each front facade;
 - b. Minimum weather protection at least five feet in depth, along at least 80 percent of the facade width, including building entries; and
 - c. Maximum front yard setback is 15 feet.
 - d. Surface parking along Boulevard Streets shall not be more than 50 percent of the site frontage. Parking lots are not allowed at street corners. No parking or vehicle circulation are allowed between the right-of-way and the building front facade. Sites with less than 100 lineal feet of frontage are exempt from this standard. See 20.92.070(E)(2) for parking lot landscape standards.

~~a. Developments abutting Boulevard Streets have the option of using Storefront Street or Green Link Street standards or a combination of both standards.~~

Comment [p10]: Greenlink street standards are inappropriate for Boulevard st other than greater setbacks, e



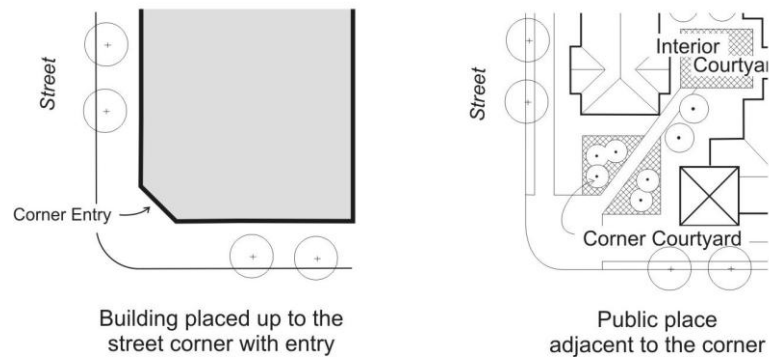
Landscaped yards

C. ~~Street-Corner Sites~~

- 1. All development proposals located on street corners ~~and Through-connection~~ sites shall include one of the following three design treatments on both sides of the corner.
 - a. Locate a building within 15 feet of the street corner. All such buildings shall comply with building corner standards in paragraph (2) below;
 - b. Provide public places, as set forth in SMC 20.92.070(F) at the corner leading directly to building entries; or

Comment [p11]: Added because a Trough-connection will create corners.

- c. Landscape 20 feet of depth of Type II landscaping for the length of the required building frontage. Include a structure on the corner that provides weather protection or site entry. The structure may be used for signage (SMC 20.92.100).



Corner Developments

- 2. Corner buildings on Boulevard and Storefront Streets using option 1.a above shall provide at least one of the elements listed below for 40 lineal feet of both sides from the corner:
 - a. 20-foot beveled building corner with entry and 80% of the first floor in transparent glass (included within the 80 lineal feet of corner treatment).
 - b. Distinctive façade (i.e. awnings, materials, offsets) and roofline design above the standards for these items in other code section of Town Center.
 - c. Balconies on all floors above the ground floor.
 - ~~d. Minimum 15-foot dimension sculpture or building-mounted artwork~~
 - ~~e. —~~Other unique treatment as determined by the Director.
- 3. Corner buildings on Green Link Streets and Through-connections using option 1.a above shall minimally provide 10-foot beveled building corners.
- ~~3.4.~~ Corner buildings on a combination of Green Link Streets or Through-connections and Boulevard or Storefront Streets shall meet the minimum requirements on their side of the corner.

Comment [p12]:

Comment [p13]: Added to further distinguish from the remainder of the façade.

Comment [p14]: New to Add Green Link and Through-connections but with scaled down requirements.

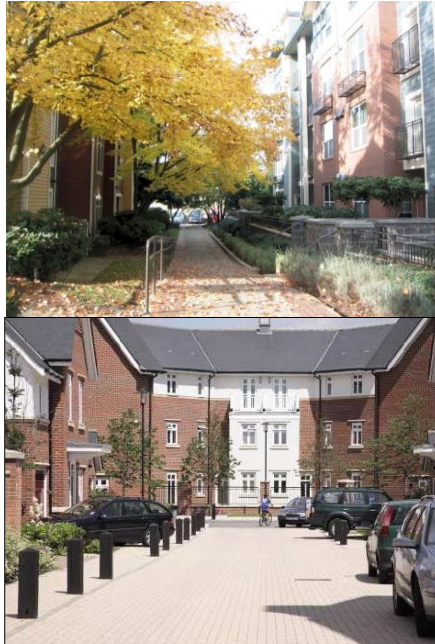


Building corners

D. Through-connections and Walkways

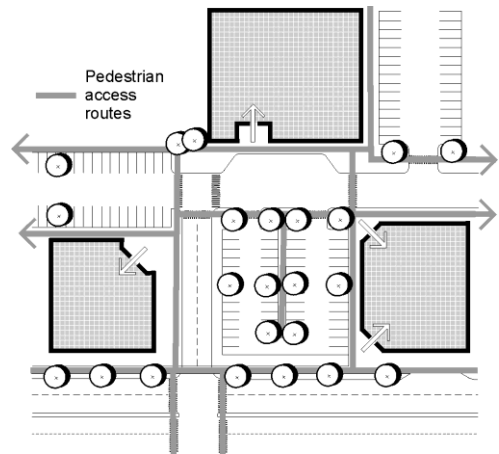
1. Developments shall include internal walkways that connect building entries, public places, and parking areas with the adjacent street sidewalks and Interurban Trail. A public easement for pedestrian access through properties and city blocks between streets shall be provided for Through-connections, as generally illustrated in the Town Center Concept Plan (SMC 20.92.030).

Walkways and Through-connections shall be connected and may be combined as long as standards of both can be met. The east-west connection aligned with N.180th may be a combination of vehicle access or street and a pedestrian Through-connection. North-south connections can be used as alley access or as a Storefront Street.



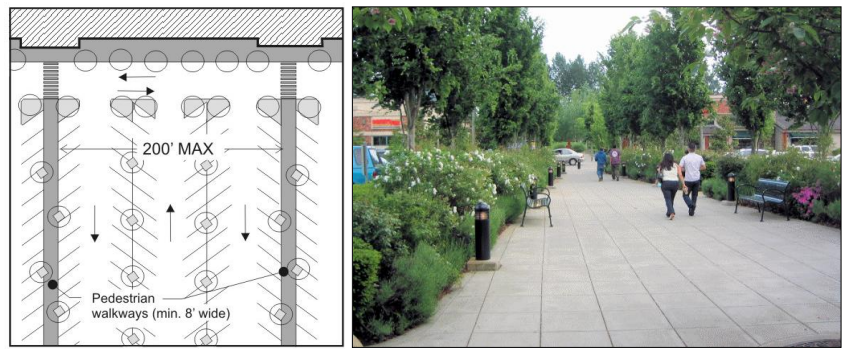
Through-connections

- a. All buildings shall have visible, clear, and illuminated walkways between the main building entrance and a public sidewalk. The walkway shall be at least eight feet wide;
- b. Continuous pedestrian walkway shall be provided to the entries of all businesses and the entries of multiple commercial buildings;
- c. For sites abutting underdeveloped land, the Director may require walkways and Through-connections stub-outs at property lines so that future, adjoining development can connect with the pedestrian system;



Well-connected walkway network

- d. Raised walkways at least 8 feet in width shall be provided for every three, double-loaded aisle or every 200 feet of parking area. Walkway crossings shall be raised a minimum 3 inches above drives;
- e. Walkways shall conform to the Americans with Disabilities Act (ADA); and



Parking lot walkway

- f. Internal walkways along the front facade of buildings 100 feet or more in length must meet Storefront or Boulevard Street standards set forth in SMC 20.92.060(C).



Internal walkways adjacent to storefronts should be designed to look and function like public sidewalks, including walkway widths and amenity areas.

- g. Deciduous street-rated trees shall be provided every 30 feet on average in grated tree pits if the walkway is 8 feet wide or in planting beds if walkway is greater than 8 feet wide. Pedestrian scaled lighting shall be provided.

E. Vehicle Parking and Landscaping

1. Minimum Off-street Parking

Parking shall be provided at the following rate:

- a. Residential —~~75~~ 1.5 spaces studio or 1+ bedroom, 1.8 spaces for 2 bedrooms, and 2.0 for 3 bedrooms or more.
- b. Retail/Office — 1 space / 4300 net square feet.
- ~~c. Civic / Office — 1 space / 500 net square feet.~~

Reductions up to 50 percent may be approved by Director using combinations of the following criteria.

- a. On-street parking along the parcel's street frontage.
- ~~d. A transit stop within ¼ mile radius.~~
- ~~e. An off-street public parking lot within ¼ mile radius.~~
- ~~f.d.~~ Shared parking agreement with adjoining parcels and land uses that do not have conflicting parking demand.
- ~~g.e.~~ Commute trip reduction program.
- ~~h.f.~~ Neighborhood meeting to discuss impacts of traffic and parking.
- ~~i.g.~~ High-occupancy vehicle (HOV) parking.
- ~~j.h.~~ Conduit for future electric vehicle charging spaces equivalent to the number of required handicapped parking spaces.

Comment [p15]: Changed to match current city standards.

Comment [p16]: Removed criteria that all TC can meet.

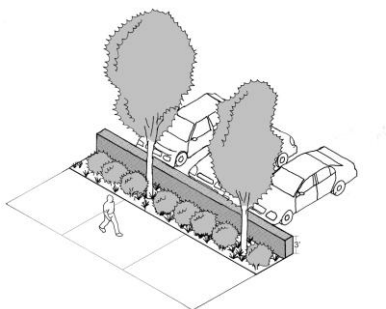
Comment [p17]: Removed toothless gesture. Redundant of 20.92.040.E

2. Parking lot landscaping

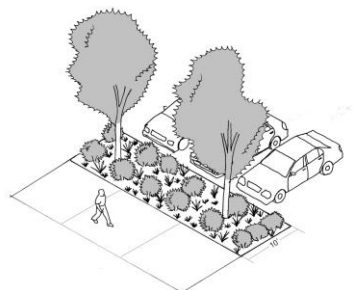
The following provisions shall supplement the landscaping standards set forth in Subchapter 7 of SMC 20.50.450. Where there is a conflict, the standards herein shall

apply. All parking lots, vehicle display, and loading areas shall meet the following requirements.

- a. Provide a 5-foot wide, Type II landscape that incorporates a continuous masonry wall between 3 and 4 feet in height. The landscape shall be between the public sidewalk or residential units and the wall (see SMC 20.50.460 for details); or
- b. Provide at least 10-foot wide, Type II landscaping.
- c. Vehicle display areas are not required to landscape with trees or shrubs. Transparent security fencing is permitted up to 8 feet. Fencing shall be made of metal other than chain link, razor, barbed, or cyclone material.
- d. Trees shall be placed interior to parking lots at a ratio of one every 10 parking spaces in curbed planters with a minimum dimension of 5 feet.
- e. All parking lots shall be separated from residential development by the required setback and planted with Type I landscaping.



2a. Parking lot planting buffer with low wall.



2b. 10-foot parking lot buffer with Type II landscaping.

F. Public Places

1. Public places are required on parcels greater than ½ acre with commercial or mixed use development at a rate of 1,000 square feet per acre. Public places may be covered but not enclosed. This standard can also be used to meet the standards of walkways as long as the function and minimum dimensions of the public place are met.
2. On parcels greater than 5 acres;
 - a. Buildings border at least two sides of the public place;
 - b. The public place shall be at least 5,000 square feet with no dimension less than 40 feet; and
 - c. 80 percent of the area shall be with surfaces for people to stand or sit on.
3. On parcels between 1/2 and 5 acres;
 - a. The public place shall be at least 2,500 square feet with a are required to have a minimum dimension of 20 feet; and
 - b. 80 percent of the area shall have surfaces for people to sit or stand on.
4. The following design elements are required for public places:
 - a. Physically accessible and visible from the public sidewalks, walkways, or Through-connections;
 - b. Pedestrian access to abutting buildings;
 - c. Pedestrian-scaled lighting (subsection H below);
 - d. Seating and landscaping with solar access at least half of a day, year-round; and
 - e. Not located adjacent to dumpster or loading areas.

Comment [p18]: Added minimum plaza size for this category of parcel size.



Public Places

G. Multifamily Open Space

1. All multifamily development shall provide open space.
 - a. Provide 800 square feet per development or 50 square feet per unit of open space, whichever is greater;
 - b. Other than private balconies or patios, open space shall be accessible to all residents and include a minimum 20-foot dimension including park, playground, roof-top decks or courtyards. This standard can also be used to meet the standards of walkways as long as the function and minimum dimensions of the open space are met;
 - c. Required landscaping can be used for open space if it does not prevent access or reduce the overall landscape standard. Open spaces shall not be placed adjacent to parking lots and service areas without screening; and
 - d. Open space shall provide seating that has solar access at least half of a day, year-round.



Multi-family open spaces

H. Outdoor Lighting

1. All publicly accessible areas on private property shall be illuminated as follows:
 - a. Minimum of one half-foot candles and maximum 25-foot pole height for vehicle areas;
 - b. One to two-foot candles and maximum 15-foot pole height for pedestrian areas;
 - c. Maximum of four-foot candles for building entries with the fixture placed below second floor; and
 - d. All private fixtures shall be full cut-off, dark sky rated and shielded to prevent direct light from entering neighboring property.

I. Service Areas ~~and Mechanical Equipment~~

1. All developments shall provide a designated location for trash and recycling storage and collection. Such elements shall meet the following standards:
 - a. Located to minimize visual, noise, odor, and physical impacts to pedestrians and residents;
 - b. Paved with concrete, screened, and covered in materials or colors that match the building; and
 - c. Located and configured so that the enclosure gate swing does not obstruct pedestrian or vehicle traffic nor require that a hauling truck project into any public right-of-way.



Trash/recycling closure with consistent use of materials and landscape screening.

2. Utility and Mechanical Equipment

- a. ~~Utility~~ Equipment shall be located and designed to minimize their visibility to the public. Preferred locations are off alleys, service drives, within atop, or under buildings or other locations away from the street. ~~Meters and similar~~ equipment shall not intrude into required pedestrian areas.



Utilities consolidated and separated by landscaping elements.

- ~~b. b.~~ All exterior mechanical equipment shall be screened around its entire periphery with a separate façade that uses the main building façade material and color.

~~All rooftop mechanical equipment shall be screened, or colored to be an integral element of the building and minimize visual impacts from the ground level of adjacent streets and properties.~~

Comment [p19]: Improved, more specific screening standards

20.92.070 Building Design Standards.**A. Purpose**

- Emphasize quality building articulation, detailing, and durable materials.
- Reduce the apparent scale of buildings and add visual interest.

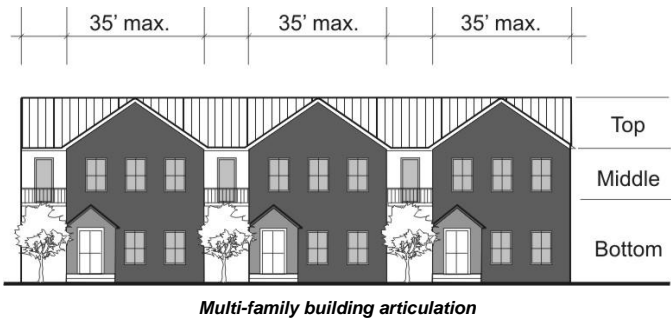
B. Façade Articulation

1. All building facing Storefront Streets per Figure 20.92.020 shall include one of the two articulation features set forth in (a) or (b) below no more than every 40 lineal feet facing a street, parking lot, or public place. Building facades less than 60 feet wide are exempt from this standard.

*Storefront articulation*

All buildings facing Boulevard Streets per Figure 20.92.020 shall include one of the two articulation features below no more than every 80 lineal feet facing a street, parking lot, or public place. Building facades less than 100 feet wide are exempt from this standard.

- a. For the height of the building, each façade shall be offset at least 2 feet in depth and 4 feet in width if combined with a change in siding materials. Otherwise, the façade offset shall be at least 10 feet deep and 15 feet wide.
 - b. Vertical piers at the ends of each façade section that project at least 2 inches from the façade and extend from the ground to the roofline.
 - c. Minimum, ground level, interior building space dimension is 12-foot height and 20-foot depth
2. All multifamily buildings or residential portion of a mixed use building facing any street shall provide the following articulation features at least every 35 feet of facade facing a street, park, and public place or open space.
 - a. Vertical building modulation 18 inches deep and 4 feet wide if combined with a change in color or building material. Otherwise, minimum depth of modulation is 10 feet and minimum width for each modulation is 15 feet. Balconies may be used to meet modulation; and
 - b. Distinctive ground or first floor façade, consistent articulation of middle floors, and a distinctive roofline or articulate on 35 foot intervals.



Multi-family building articulation

3. Roofline Modulation

Rooflines shall be modulated at least every 120 feet by emphasizing dormers, chimneys, stepped roofs, gables, or prominent cornices or walls. Rooftop appurtenances are included as modulation. Modulation shall consist of a roofline elevation change of at least four feet every 50 feet of roofline.

4. Maximum Façade

~~A building exceeding~~For each 150 feet in length along the street front a building shall have a minimum 30-foot wide section that is offset at least by 20 feet through all floors.



Façade widths using a combination of façade modulation, articulation, and window design.

5. Windows

Buildings shall recess or project individual windows above the ground floor at least two inches from the façade or ~~incorporate use~~ window trim at least four inches in width ~~or a color that contrasts with the façade color.~~



Window trim design

6. Secondary Entry

Weather protection at least 3 feet deep and 4 feet wide is required over each secondary entry;



Covered secondary public access

7. Façade Materials

- a. Metal siding shall have visible corner moldings and trim and shall not extend lower than six feet above grade. Masonry, concrete, or other durable material shall be incorporated between the siding and the grade. Metal siding shall be factory finished, with a matte, non-reflective surface.



Masonry or concrete near the ground and proper trimming around windows and corners.

- b. A singular style, texture, or color of concrete block shall not comprise more than 50 percent of a façade facing a street or public space.



The left image uses smooth gray blocks on the vertical columns and beige split-faced blocks above the awnings. The storefront in the right image uses gray split face and some lighter, square, smooth-faced blocks below the storefront windows.

- c. Synthetic stucco must be trimmed and sheltered from weather by roof overhangs or other methods and are limited to no more than 50 percent of façades containing an entry and shall not extend below 2 feet above the grade.



Concrete near the ground level and a variety of other surface materials on the façade.

8. Prohibited exterior materials.
 - a. Mirrored glass, where used for more than 10 percent of the façade area.
 - b. Chain-link fencing.
 - c. Corrugated, fiberglass sheet products.
 - d. Plywood siding.

20.92.080 Sign Design Standards.**A. Purpose**

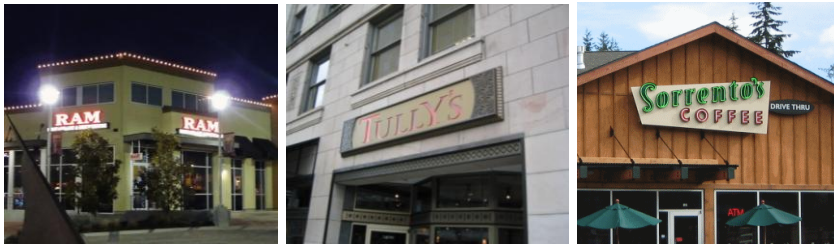
- Require signage that is both clear and of appropriate scale for the project.
- Enhance the visual qualities of signage through the use of complementary sizes, materials and methods of illumination.
- Require signage that contributes to the character of Shoreline's Town Center.

B. Applicability

The sign standards herein shall supplement the provisions of SMC 20.50.540. Where there is a conflict, the provisions herein shall apply.

C. Permitted Illumination

1. Channel lettering or individual back-lit letters mounted on a wall or individual letters placed on a raceway, where only light shines through the letters.
2. Opaque cabinet signs where light only shines through letter openings.
3. Shadow lighting, where letters are backlit, but light only shines through the edges of the letters.
4. Neon signs
5. Externally lit signs



Individual backlit letters (left image), opaque signs where only the light shines through the letters (center image), and neon signs (right image).

D. Monument Signs

1. One sign is permitted per frontage, per property, and regardless of the number of tenants. An additional monument sign is permitted on a property if the frontage length is greater than 250 feet and the signs are at least 150 feet apart.
2. Use materials and architectural design elements that are consistent with the architecture of the buildings.
3. Signs in Zone TC-3: Maximum height: 6 feet and maximum area: 50 square feet per sign face.
4. Signs in zones TC-1 and TC-2 when placed along Aurora Avenue, N. 175th or N. 185th streets. Maximum height: 12 feet and maximum area: 100 square feet per sign.

5. Signs may be placed up to the front property line if site distancing and public safety standards are met.
6. Signs shall be setback from the side property lines at least 20 feet.



Monument sign

E. Building Signs

1. Each tenant or commercial establishment is allowed one building sign - wall, projecting, marquee, awning, or banner sign per facade that face the adjacent streets or customer parking lot.
2. Building signs shall not cover windows, building trim, edges, or ornamentation.
3. Building signs may not extend above the parapet, soffit, the eave line, or on the roof of the building.
4. Each sign area shall not exceed 25 square feet for Zone TC -3 and 50 square feet for zones TC-1 and TC-2.
5. The sign frame shall be concealed or integrated into the building's form, color, and material.



Signs are centered on architectural features of the building.

6. Projecting, banner, and marquee signs (above awnings) shall clear sidewalk by 9 feet and not project beyond the awning extension or 8 feet, whichever is less. These signs may project into public rights-of-way for storefront buildings, subject to City approval.

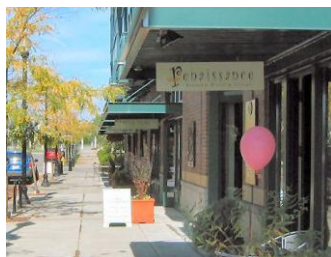


Projecting sign

F. Under-awning Signs

1. Not extend within 1-foot of the awning outer edge and the building façade;
2. Minimum clearance of 9 feet between the walkway and the bottom of the sign;
3. Not exceed 2 feet in height; and
4. One sign per business.

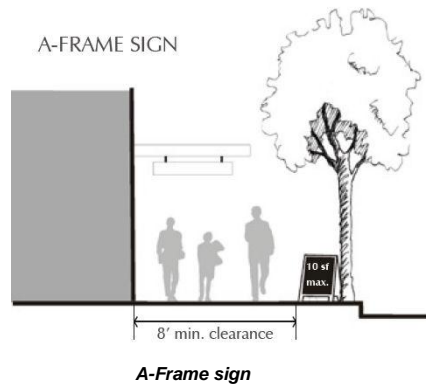
G. Windows signs are exempt from permits but cannot exceed 25 percent of the window area



Under-awning signs

H. A-Frame or Standing Signs

1. One sign per business;
2. Must be directly in front of the business;
3. Cannot be located within the 8 foot sidewalk clearance on designated Storefront Street and 5 feet on all other sidewalks and internal walkways;
4. Shall not be placed in landscaping, within 2 feet of the street curb where there is on-street parking, public walkways, or crosswalk ramps.
5. Shall not exceed 6 square feet per side; and
6. No lighting of signs is permitted.



I. Transition Overlay and Zone TC-4 Signs

All signs in the Transition Overlay and Zone TC-4 shall meet residential sign standards of SMC 20.50.540(B).

J Prohibited signs

1. Pole signs.
2. Billboards.
3. Electronic changing message or flashing signs.
4. Backlit awnings used as signs.
5. Other signs set forth in SMC 20.50.550.

Table 20.30.040 – Summary of Type A Actions and Target Time Limits for Decision, and Appeal Authority

Action Type	Target Time Limits for Decision	Section
Type A:		
1. Accessory Dwelling Unit	30 days	20.40.120, 20.40.210
2. Lot Line Adjustment including Lot Merger	30 days	20.30.400
3. Building Permit	120 days	All applicable standards
4. Final Short Plat	30 days	20.30.450
5. Home Occupation, Bed and Breakfast, Boarding House	120 days	20.40.120, 20.40.250, 20.40.260, 20.40.400
6. Interpretation of Development Code	15 days	20.10.050, 20.10.060, 20.30.020
7. Right-of-Way Use	30 days	12.15.010 – 12.15.180
8. Shoreline Exemption Permit	15 days	Shoreline Master Program
9. Sign Permit	30 days	20.50.530 – 20.50.610
10. Site Development Permit	60 days	20.20.046, 20.30.315, 20.30.430
11. Deviation from Engineering Standards	30 days	20.30.290
12. Temporary Use Permit	15 days	20.40.100, 20.40.540
13. Clearing and Grading Permit	60 days	20.50.290 – 20.50.370
14. Planned Action Determination	28 days	20.90.025
<u>15. Design Review</u>	<u>28 days</u>	<u>20.30.297</u>

An administrative appeal authority is not provided for Type A actions, except that any Type A action which is not categorically exempt from environmental review under Chapter 43.21C RCW or for which environmental review has not been completed in connection with other project permits shall be appealable. Appeal of these actions together with any appeal of the SEPA threshold determination is set forth in Table 20.30.050(4). (Ord. 531 § 1 (Exh. 1), 2009; Ord. 469 § 1, 2007; Ord. 352 § 1, 2004; Ord. 339 § 2, 2003; Ord. 324 § 1, 2003; Ord. 299 § 1, 2002; Ord. 244 § 3, 2000; Ord. 238 Ch. III § 3(a), 2000).

20.30.297 Design Review (Type A)

Design Review approval shall be granted by the Director upon his/her finding that:

1. The design meets the requirements of the applicable code subsections.
- ~~2. The design improves the function, continuity, connection, or pedestrian interest from building to building or site to site.~~
- ~~3. The choice of materials and architectural elements is compatible with the context of other development in the vicinity.~~
4. Departures from the design standards in the applicable chapter shall be consistent with the purposes or intent of each subsection or be justified due to unusual site constraints so that meeting the design standards represents a hardship to achieving full development potential.
 - ~~a. For the Town Center District, d~~imensional standards in Table 20.92.030 regarding setbacks and building envelope cannot be ~~departed from modified by Design Review in the Town Center District.~~
 - ~~a-b. No departure from standards is allowed in Transition Overlay and the TC-4 zone.~~

Comment [p20]: These qualities are difficult to define but are consistent with the intent of the development standards proposed.

Comment [p21]: "Compatibility" and "context" may be unintended and undesirable if it means current development that does not meet the purposes of the code.

Comment [p22]: To remove unpredictable departures in heights and setbacks

Comment [p23]: To protect single family neighborhoods.

20.50.021 Development in the mixed-use zone (MUZ)

Development in the MUZ zone shall meet the following requirement:

- A. All developments in the MUZ zone are subject to Design Review Approval in SMC 20.30.297.

20.91.040 Design review. (Ridgecrest Planned Area)

- A. **Applicability.** Design review will be required for developments in Ridgecrest Commercial Planned Area 2 that are 1.5 acres or more and that meet one of the thresholds in SMC 20.50.125.

Standards for Approval. When design review is required, the applicant will demonstrate that plans satisfy the criteria in SMC 20.30.297.

- ~~B.~~ **Design Departures.** A permit applicant wishing to modify any of the standards in this chapter may apply for a design departure under SMC 20.30.297

20.20 Definitions.

The following definitions apply to Chapter 20.

Building articulation	The emphasis to architectural elements (like windows, balconies, entries, etc.) that create a complementary pattern or rhythm, dividing large buildings into smaller identifiable pieces. See SMC 20.92.180 for applicable standards.
Banner sign	A sign constructed of cloth, canvas, or other similar light weight material that can easily be folded or rolled, but does not include paper or cardboard.
Boulevard Street	Refers to a street and/or segment of a street where there's an option for commercial storefronts or landscaped setbacks along the street with the option of ground floor residential or commercial uses.
Frontages	Facilities between the curb and private development along streets – typically curbs, amenities, and sidewalks.
Green Link Street	Refers to a street and/or segment of a street envisioned to have or maintain landscaped building setbacks along the street. See Figure 20.92.030 for the location of designated Landscaped Streets and SMC 20.92.070(B)(3) for the description and applicable standards for properties fronting on designated Landscaped Streets.
Modulation	A stepping back or projecting forward of portions of a building face, within specified intervals of building width and depth, as a means of breaking up the apparent bulk of a structure's continuous exterior walls.
Public places	See SMC 20.92.140 for the description, standards, and guidelines for public places.
Roofline Modulation	Refers to a variation in roof form. See SMC 20.92.180 for provisions.
Storefront	A pedestrian-oriented façade placed up to the edge of a public sidewalk. See SMC 20.92.070(C)(1).
Storefront Street	Refers to a street or segment of a street where envisioned to have storefronts placed up to the edge of the sidewalk. See figure 20.92.030 for the location of designated Storefront Streets and SMC 20.92.070(B)(1) for the description and applicable standards for properties fronting on designated Storefront Streets.
Transparent window	A window that is capable of transmitting light so that objects or images can be seen as if there were no intervening material variation in roof form.

Trellis	A frame supporting open latticework used as a screen or a support for growing vines or plants.
Walkways	On-site hard surfaces for pedestrian and non-motorized circulation.

David Levitan

From: Michael U. Derrick [mderrick@ronaldwastewater.org]
Sent: Wednesday, May 11, 2011 10:24 AM
To: David Levitan
Subject: Fwd: Town Center subarea Plan

David: This is more appropriately sent to you rather than Paul.
Michael

----- Original Message -----

Subject: Town Center subarea Plan
Date: Tue, 10 May 2011 13:03:08 -0700
From: Michael U. Derrick <mderrick@ronaldwastewater.org>
Organization: Ronald Wastewater District
To: pcohen@shorelinewa.gov
CC: Mark Relph <mrelph@shorelinewa.gov>

Paul: I noticed this text in the subarea plan: "The City of Shoreline also expects to acquire the Ronald Wastewater District by 2016, which should result in wastewater review being even better incorporated into the City's development review process." (pg 55) This is not correct. The interlocal agreement expires in 2017.

Recently, the City Manager responded this way to a question regarding the acquisition of RWD: "She was also asked whether the city will eventually take over Ronald Wastewater, and she said that there is an agreement to look into the wisdom of such a plan after 2017."

By the way, let's not wait until 201? to "even better" incorporate wastewater review into the City's development review process. We're open to ideas to improve right now!

Michael

--
Michael U. Derrick
General Manager
Ronald Wastewater District
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www.ronaldwastewater.org
(206) 546-2494 (Phone)
(206) 546-8110 (Fax)

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CITY OF SHORELINE
Town Center Subarea Planned Action
Draft Supplemental Environmental Impact Statement

The DSEIS is available on the City of Shoreline website:

[Planned Action Draft SEIS](#)

Contact David Levitan at (206) 801-2554 for more information.